

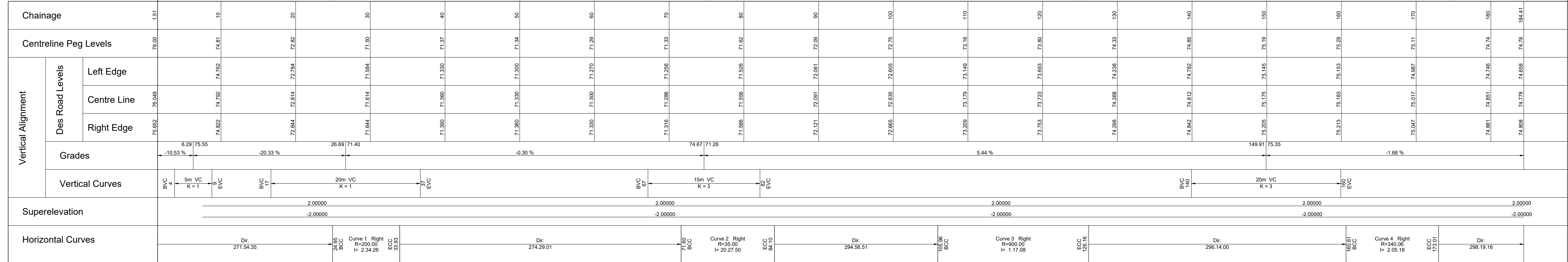
KEY

NGL  
ROAD

HOR. SCALE 1:250

VER. SCALE 1:100

Datum 64.00



Road1-1 &amp; Road 1-2 - Ch. 1 to 184

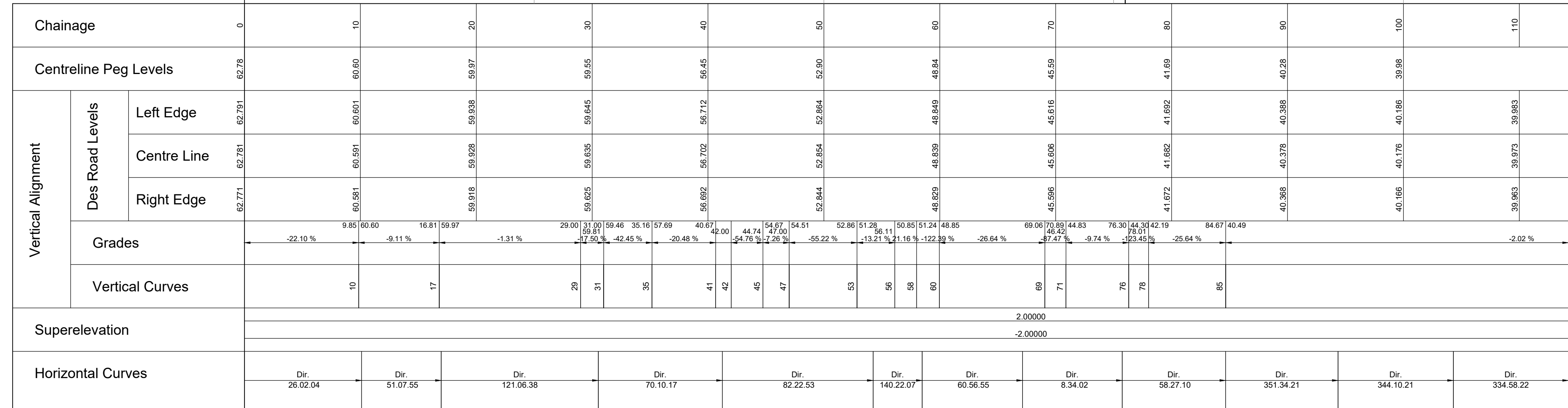
KEY

NGL  
FOOTPATH  
STAIRCASE

HOR. SCALE 1:250

VER. SCALE 1:100

Datum 44.00



Footpath3 - Ch. 0 to 135

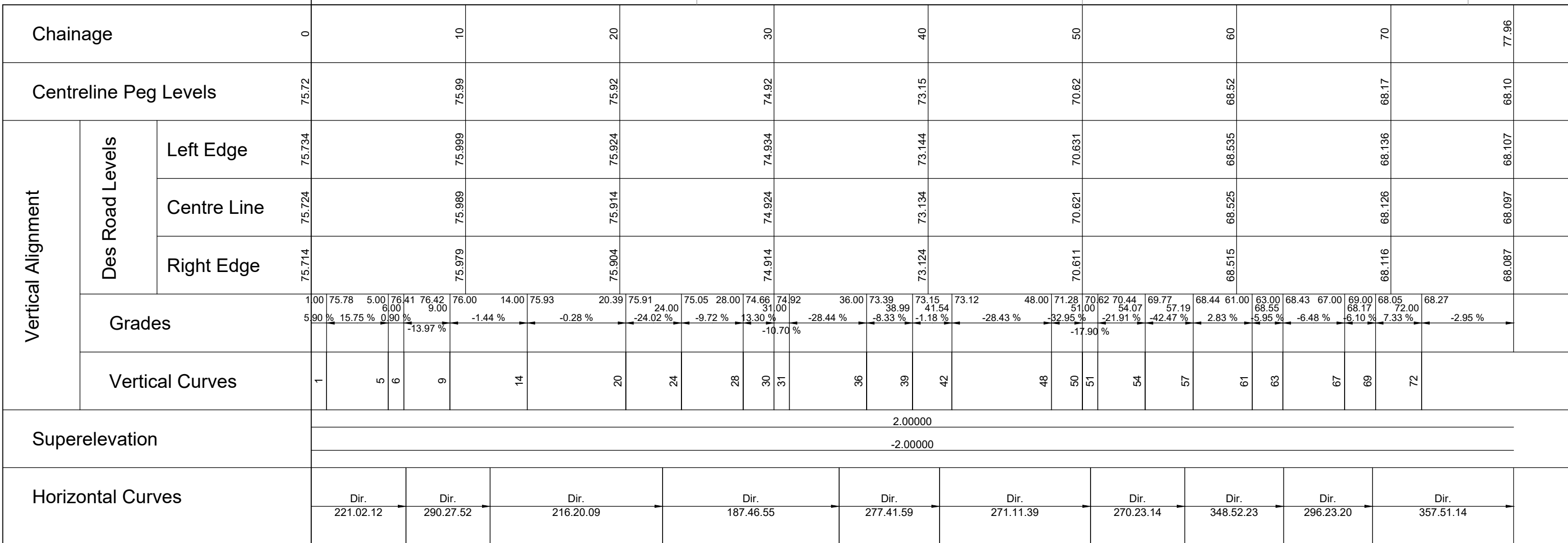
KEY

NGL  
FOOTPATH  
STAIRCASE

HOR. SCALE 1:250

VER. SCALE 1:100

Datum 62.00



Footpath2 - Ch. 0 to 77

## NOTES

GENERAL :

1. PROVE ALL SERVICES PRIOR TO CONSTRUCTION.
2. ALL LEVELS AND DIMENSIONS TO BE VERIFIED ON SITE.
3. ALL SETTING OUT TO BE UNDERTAKEN BY A REGISTERED PROFESSIONAL LAND SURVEYOR.
4. ALL SURVEY AND SETTING OUT DATA PROVIDED IS BASED ON (WGS 84).
5. ALL WORK AREAS TO BE REINTEGRATED (PREMIUM CONCRETE ETC.) MUNICIPALITY TO EXECUTE ALL CONNECTIONS INTO MUNICIPAL LINES.
6. UNLESS OTHERWISE AGREED WITH ENGINEER, CONTRACTOR TO SUPPLY ENGINEER WITH RESULTS OF COMPACTION TESTS, AND WHEN APPLICABLE, PERCENTAGE STABILISATION TESTS ON BACKFILL.
7. ALL WORKS IN ACCORDANCE WITH CITY OF DURBAN SPECIFICATIONS AND SANS 1200.
8. THE ENGINEER REQUIRES 24 HOURS NOTICE FOR ALL INSPECTIONS.

## MATERIAL COMPLIANCE TESTING :

1. RESULTS OF COMPACTION AND CBR TESTS ON IN-SITU SUB-BASE MATERIAL AND FILLED SUB-BASE MATERIAL MUST BE SUBMITTED TO THE ENGINEER FOR APPROVAL BEFORE THE UPPER LAYER WORKS ARE IMPORTED TO THE SITE AND PLACED.

## FREQUENCY OF TESTS :

1. CBR TESTS 1 PER 300M<sup>2</sup>.
2. COMPACTION TESTS 1 PER 200M<sup>2</sup> NOTE: COMPACTION TESTS WILL BE REQUIRED FOR EACH OF THE VARYING LAYER THAT IS IMPORTED AND COMPACTED IN PLACE.

## RETAINING WALL :

1. ALL LEVELS, DIMENSIONS AND SETTING OUT DETAILS TO BE VERIFIED BY CONSULTANT AND CONTRACTORS ON SITE PRIOR TO CONSTRUCTION.
2. THE POSITIONS OF ACCESSES ARE TO BE DETERMINED IN CONSULTATION WITH THE LOCAL COMMUNITY. DAYLIGHT REQUIREMENTS ARE TO BE DECIDED BY THE ENGINEER ON SITE. CONCRETE WEDGES ACCORDING TO CIVIL ENGINEERS DETAILS AND SPECIFICATIONS MAY BE USED IN PLACE OF SURFACED BELL-MOUTHS FOR ACCESSES SERVING SINGLE RESIDENTIAL PROPERTIES.
3. EXISTING ROAD SIGNS, SERVICES AND FENCING AFFECTED BY CONSTRUCTION ARE TO BE REMOVED/RELOCATED ON INSTRUCTION BY THE CIVIL ENGINEER.
4. UNDERGROUND SERVICE CROSSINGS AND MARKERS ARE TO BE ACCORDING TO CIVIL ENGINEERS DETAILS AND SPECIFICATIONS.
5. ALL NEW ROAD SIGNS AND ROAD MARKING REQUIREMENTS ARE TO CONFORM TO THE SOUTHERN AFRICAN DEVELOPMENT COMMUNITY ROAD TRAFFIC SIGNS MANUAL (SADC - RTSM).
6. ALL WORK IS TO BE CARRIED OUT IN ACCORDANCE WITH "COLT" SPECIFICATIONS FOR ROAD AND BRIDGE WORKS FOR STATE ROAD AUTHORITIES.
7. NEW FILLS AND EXPOSED CUTS ARE TO BE TOP-SOILED AND VEGETATED IMMEDIATELY AFTER CONSTRUCTION TO PREVENT EROSION.

## ROADS AND FOOTPATHS :

1. ALL LEVELS, DIMENSIONS AND SETTING OUT DETAILS TO BE VERIFIED BY CONSULTANT AND CONTRACTORS ON SITE PRIOR TO CONSTRUCTION.
2. THE POSITIONS OF ACCESSES ARE TO BE DETERMINED IN CONSULTATION WITH THE LOCAL COMMUNITY. DAYLIGHT REQUIREMENTS ARE TO BE DECIDED BY THE ENGINEER ON SITE. CONCRETE WEDGES ACCORDING TO CIVIL ENGINEERS DETAILS AND SPECIFICATIONS MAY BE USED IN PLACE OF SURFACED BELL-MOUTHS FOR ACCESSES SERVING SINGLE RESIDENTIAL PROPERTIES.
3. EXISTING ROAD SIGNS, SERVICES AND FENCING AFFECTED BY CONSTRUCTION ARE TO BE REMOVED/RELOCATED ON INSTRUCTION BY THE CIVIL ENGINEER.
4. UNDERGROUND SERVICE CROSSINGS AND MARKERS ARE TO BE ACCORDING TO CIVIL ENGINEERS DETAILS AND SPECIFICATIONS.
5. ALL NEW ROAD SIGNS AND ROAD MARKING REQUIREMENTS ARE TO CONFORM TO THE SOUTHERN AFRICAN DEVELOPMENT COMMUNITY ROAD TRAFFIC SIGNS MANUAL (SADC - RTSM).
6. ALL WORK IS TO BE CARRIED OUT IN ACCORDANCE WITH "COLT" SPECIFICATIONS FOR ROAD AND BRIDGE WORKS FOR STATE ROAD AUTHORITIES.
7. NEW FILLS AND EXPOSED CUTS ARE TO BE TOP-SOILED AND VEGETATED IMMEDIATELY AFTER CONSTRUCTION TO PREVENT EROSION.

## STORMWATER :

1. ALL STORMWATER PIPES ARE CLASS 1000 CONCRETE AND HD CL 34 UPVC PIPES.
2. ALL STORMWATER CONCRETE PIPES TO COMPLY WITH SANS 677 STANDARDS.
3. ALL UPVC PIPES TO COMPLY WITH SANS 969 STANDARDS.
4. ALL JOINTS TO BE SPRIGOT AND SOCKET TYPE.
5. ALL STORMWATER PIPES TO BE LAID ON CLASS 8 BEDDINGS.
6. ALL EXISTING DRAINAGE CULVERTS ARE TO BE INSPECTED, AND ANY FOUND IN UNSERVICEABLE CONDITION ARE TO BE REPLACED ON INSTRUCTION BY THE CIVIL ENGINEER.
7. CULVERT INVERTS ARE TO BE DECIDED BY CIVIL ENGINEER ON SITE UNLESS SHOWN OTHERWISE. MIN. COVER = 600mm. MIN. SLOPE = 2%.

SCALE (mm)

0 10 20 30 40 50 60 70 80 90 100

No.

REFERENCE DRAWINGS

REV	DESCRIPTION	BY	DATE
P1	ISSUED FOR TENDER	Z.M	2023.05.30

Professional person S.SIRUPATHY P.TechEng Registration 201470135

CLIENT

PROJECT  
eTHEKWINI INCREMENTAL  
SERVICES GUMTREE ROAD,  
KENNIE, WARD 34

DETAILS

ROAD/FOOTPATH  
LONG SECTIONS -  
SHEET 1

SUITE ESS106 STRATHMORE PARK, 305 MUSGRAVE ROAD, MUSGRAVE, DURBAN

e-mail: admin@mapafrica.co.za

web: www.mapafrica.co.za

FAX (031) 3092929 TEL. (031) 3095831

DESIGNED	Z.M	COPYRIGHT RESERVED	SCALE
DRAWN	Z.M	AS SHOWN	
APPROVED	S.S	PL	DATE 2023.02.24
DRAWING No.	557/GUMT-PH2/901	REV	P1

FOR TENDER